

WHEAT CITY GOLF COURSE

MASTER PLAN REPORT



Prepared by :

Ross Mitchell, MES, MCIP, RPP

SISON BLACKBURN CONSULTING INC.

r.mitchell@sbcinc.ca

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April 18, 2018



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Consultant contributors to this report included:

- City of Brandon, Robert Zilke, Community Planner
- Neil Cooper Architect, Neil Cooper, Project Architect
- John White Architect, John White, Architect of Record
- Newton Engineering, Glen Newton, Site Civil Design
- WSP, Maury Steindel, Traffic Impact Study
- MORR Transportation Consulting, Steven Wood, Parking Management Plan
- M. Block & Associates Ltd., Jeffrey Block, Site Geotechnical
- Horizon Builders Ltd., Rod Lindenbergs, Public Engagement

MASTER PLAN CONTEXT

1.1 REASON FOR THE MASTER PLAN: This master plan provides information and policy support for the development of a portion of the Wheat City Golf Course, also known as the Brandon Recreation Centre (BRC). The major project components include construction of a 68-unit condominium development, named Riverside Estates, redevelopment and expansion of the existing clubhouse including the conversion of the curling rink to a hockey rink, and the construction of a new commercial building along with parking and site circulation improvements. The project is being undertaken by Horizon Builders Ltd., who was the successful respondent to a 2016 Request for Proposals issued by the City of Brandon for the acquisition and development of lands deemed surplus to golf course requirements. The project will benefit the BRC by enlivening the golf course and associated amenities, creating a vibrant, year round recreation-focused residential environment, and providing the City needed tax revenue to assist in golf course maintenance.

1.2 ENABLING POLICY: The Wheat City Master Plan has been prepared in accordance with Section 9.0 of the Brandon and Area Planning District (BAPD) Development Plan. Section 9 includes policies related to the development of areas of Brandon designated as Special Development Sites (SDS); a designation that applies to the portion of the Wheat City Golf Course lands containing the subject lands that "*may be considered for development that is compatible with the recreation use and adjacent uses.*" Section 9.2.8. Appendix "A" provides a completed copy of the City of Brandon SDS Master Plan Checklist.

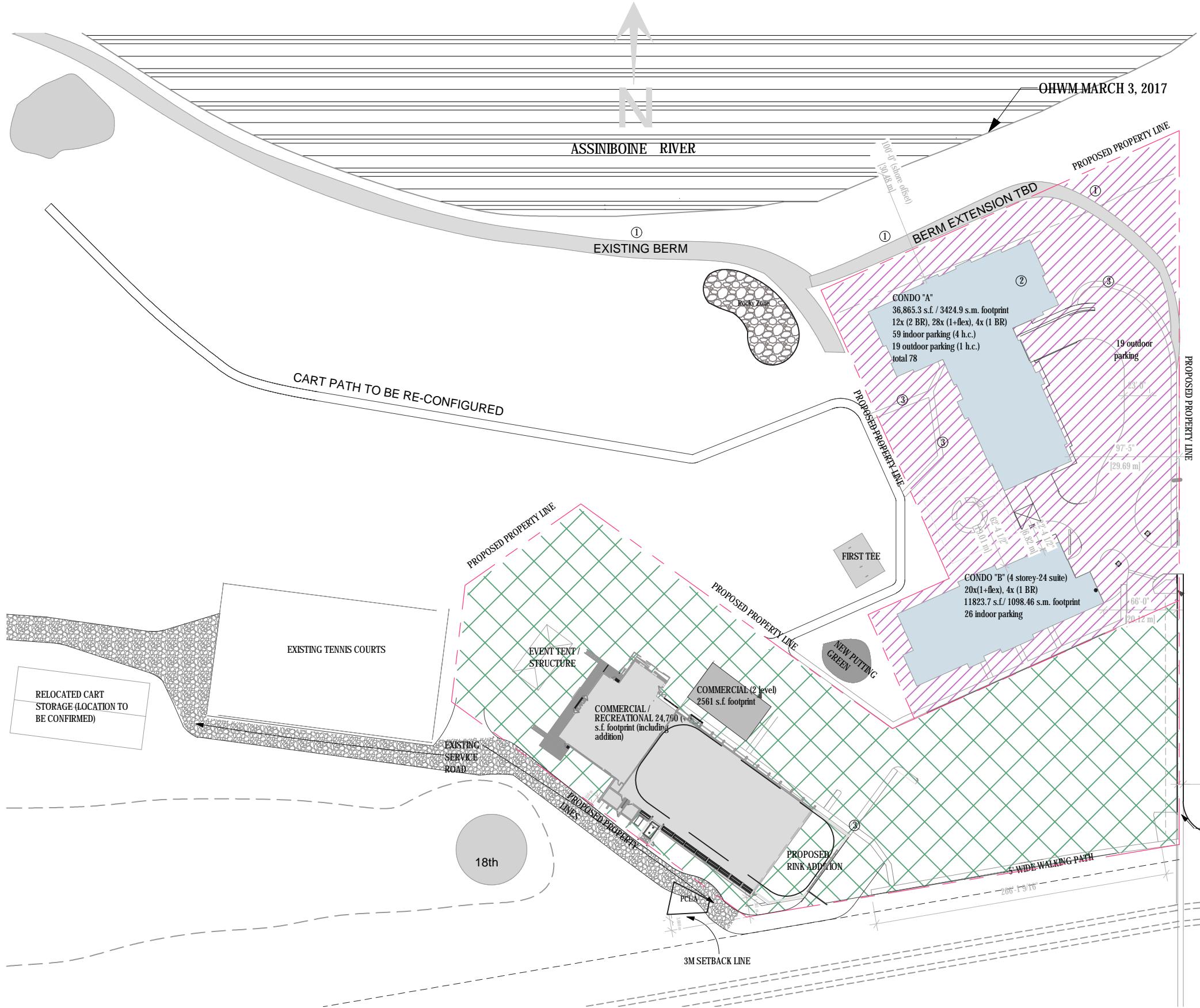
1.3 PLANNING AREA: The SDS applies to the approximately 2.3 hectares (5.69 acres) of land extending from the CP tracks to the river encompassing the subject condominium site as well as the golf course club house, parking lot and cart shed. Figure 1 shows the boundary of the Wheat City Golf Course Special Development Site hereafter referred to as the Master Plan planning area.

Figure 1: Master Plan Planning Area & Parcel Plan

1.4 MASTER PLAN FORMAT: This Master Plan report presents information on the Riverside Estates condominium project and details how the project aligns with the City's planning, parks and recreation, environmental, transportation, and site design policies, regulations and guidelines. The report sections deal with the major project components and refers to the relevant by-laws applying to each component. For ease of reference, Appendix B provides a table that lists the relevant policies and regulations and indicates using point-form text how the planning and development criteria are addressed in the project's architectural design, site planning, transportation access, engineering services, and community integration.

1.5 SUPPORTING DOCUMENTATION: The following studies, memos and City of Brandon check-lists have been prepared in support of the Wheat City Golf Course Master Plan and are included as appendices to this document and summarized under the pertinent sections of the main body:

- SDS Master Plan Check List
- Policy Reference Table
- Traffic Impact Study
- Low Impact Design Check List
- Municipal Servicing Report
- Geotechnical Analysis
- Public Outreach Memorandum
- Parking Management Plan



Site - Figure 1

Scale: 1 = 100' 0

Drawing Title:

Figure 1 Planning Area & Parcel Plan

Riverside Estates

3500 McDonald Brandon, MB

Jill Cooper Architect

109-374 River Avenue, Winnipeg, Manitoba R3L 0E4

109- 374 River Avenue, Wpg, Manitoba R3L 0E4

Mar 28, 2018

A1.0

DEVELOPMENT OVERVIEW

2.1 PROJECT DESCRIPTION: The Wheat City Golf Course Project is located on land to be subdivided from the City's golf course property. Figure 2 on the following page shows the site plan for the project. The development is located on approximately 2.3 hectares (5.69 acres) of land subdivided from the City's golf course property into two (2) lots. The project will result in the construction of three new buildings and the relocation of the cart shed in order to expand the parking area. Proposed Parcel "A" will contain the two condominium apartment buildings totaling 68 units on 1.047 ha. of land (2.58 ac.). Proposed Parcel "B" is 1.251 ha. (3.11 ac.) in area and will contain existing arena and golf course club house along with the proposed 475.8 square metre (5,122 square foot), two-storey commercial building housing the relocated pro shop along with up to 7 commercial rental units (CRUs). Plans are to renovate the restaurant and lounge and to expand the arena so that the ice surface can accommodate regulation youth play. The specific project components are detailed in later sections of this Master Plan document.

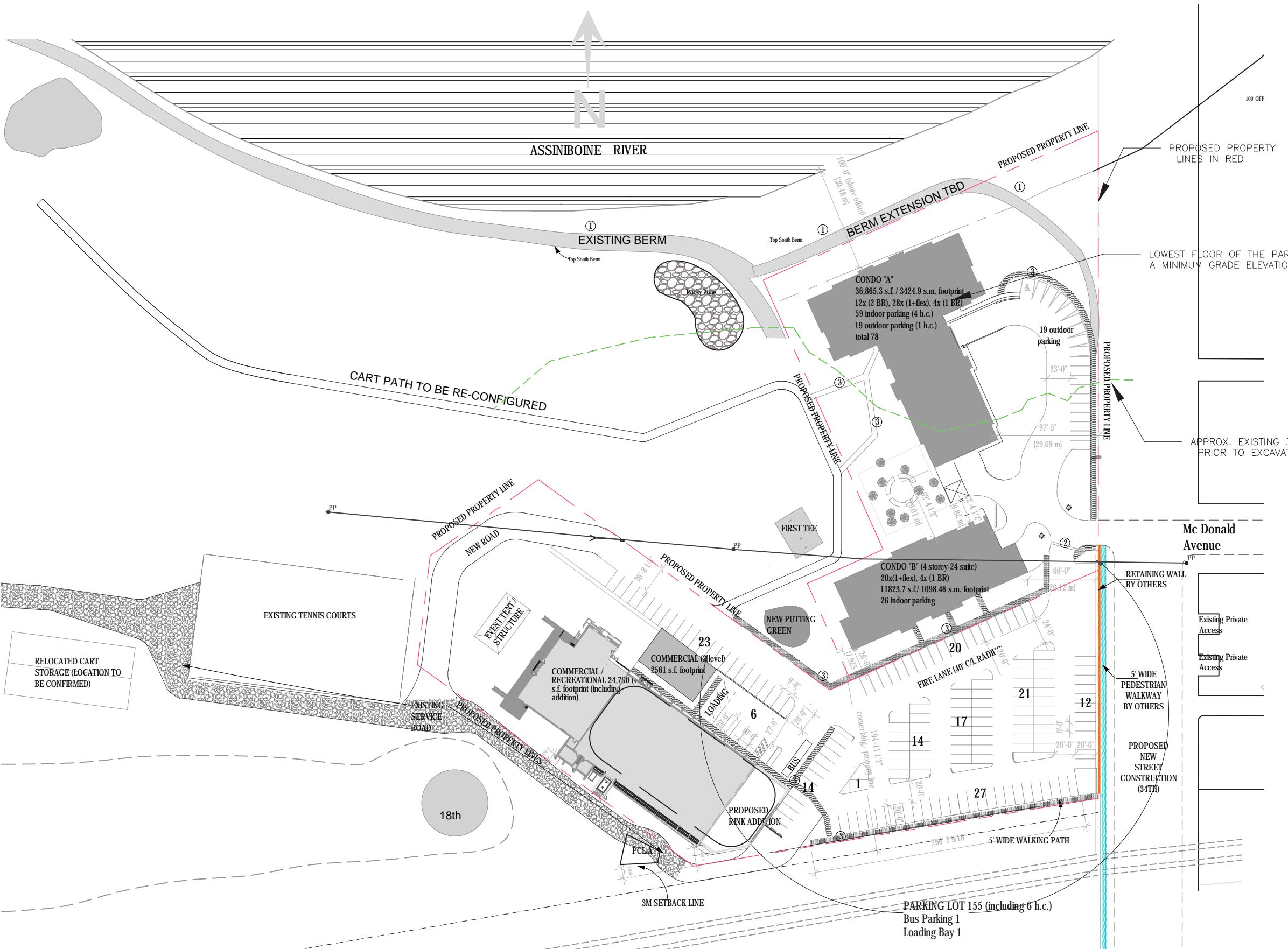
Figure 2: Site Plan

2.1 NEIGHBOURHOOD CONTEXT: Figure 3 on the following page is a Google Earth image showing the neighbourhood context and roughly outlines the Special Development Site. The project is located at the west end of McDonald Avenue in the northwest corner of the Assiniview Park neighbourhood, one of the City's older residential areas. The project is separated from the larger neighbourhood to the south by the CPR mainline. As a condition of the project, the City will extend 34th Street southward across the tracks to connect to McDonald Avenue in order to provide primary access to the development and the recreational amenities. In the meantime, McDonald Avenue will provide access to the site from 26th Street and 18th Street, two of Brandon's major arterial streets.

The McDonald Avenue neighbourhood is a low-density residential area. The predominant housing format is single-family although the housing inventory does include infill housing consisting of semi-detached and multi-plex dwellings. Several of the original houses on the street have been demolished to make way for new homes. Original homes on the south side of McDonald are especially susceptible to replacement as these lots back onto the Assiniboine River. The closest housing to the subject development is a single-family bungalow on the south side of McDonald, approximately 75 metres (246 feet) through forest from the northerly apartment building, and a 4-plex condominium on the north side of McDonald approximately 54.0 metres (177 feet) from the southerly apartment building across the 34th Street ROW.

2.3 SITE DESCRIPTION: The site topography declines gradually in a northeasterly direction towards the river over the southerly or "upper" portion of the site to the area of the 1st hole T-boxes, in line with the south limit of McDonald. Thereafter, the site slopes abruptly onto the floodplain and river's edge, the "lower" portion of the site. The T-boxes are being relocated and the site contoured in order to accommodate the condominium buildings.

The most prominent topographic feature is a steep embankment along the east limit of the site sloping down to the floodplain. The embankment and floodplain below were historically cloaked in riverbottom forest however the recent flooding has ravaged the vegetation to the point where the area would benefit from the removal of dead and downed trees and scrub vegetation consisting of invasive plant varieties. Vegetation cover over the remainder of the site consists largely of manicured grass although mature trees do dot the area in the vicinity of the arena and along the tracks. Approximately 35% of the site could be considered in a "natural" state, essentially the NE portion of the site delineated by the embankment and floodplain. The topography and elevations in this area essentially prohibit redevelopment. The balance of the site has been greatly altered over the years through golf course



Site Plan

Drawing Title:
Site Plan
Riverside Estates
2500 McDonald, Brandon, MB

Neil Cooper Architect

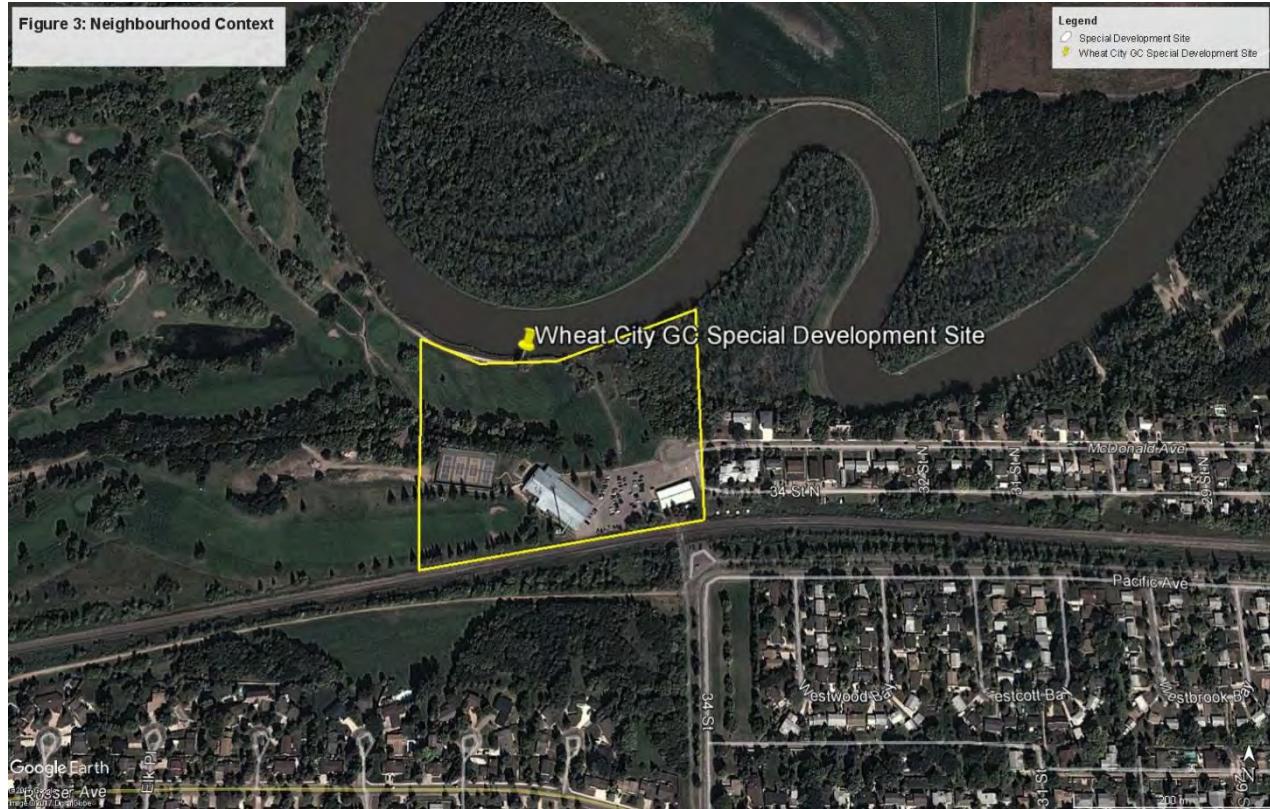
109- 374 River Avenue, Wpg, Manitoba R3L 0E4

April 19 2018

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development and it is within this larger, disturbed portion of the site where the proposed development will principally be located.

Figure 3: Neighbourhood Context



The upper portion of the site is currently used for golf course access and parking and contains the cart storage shed which will be relocated to west of the tennis courts. The driveway to the club house/arena building and parking area are paved with asphalt. The current driveway access will be retained in its present location with separate approaches added for driveways leading to the condominium building entrances and underground parking garages.

Stormwater and spring melt runoff from the paved areas and upper portion of the site is “informal,” occurring as “sheet drainage,” meaning it follows the contours of the hard and soft surfaces primarily in a northeasterly direction flowing down the escarpment to the river. Some runoff from the site runs to the northwest where it is picked up by a swale running along the southern edge of the first fairway that empties into a pond which is drawn down when necessary by pumping into the river.

REGULATORY FRAMEWORK

The planning and design of the proposed Wheat City condominium and site redevelopment project invokes policies, guidelines, regulations and recommendations from a number of Brandon planning documents and by-laws related to urban design and development. The following sections identify these documents and their relevance to the Wheat City proposal.

3.1 BRANDON & AREA PLANNING DISTRICT DEVELOPMENT PLAN (2013): Section 9.3 of the Development Plan states the intent of Special Development Area Master Plans and sets out guidelines for their preparation. The overall objective of the Special Development DS Master Plan is to “*ensure future development is logical and well-integrated with existing land uses on the site and in adjacent areas, proper linkages are made, traffic and access routes analyzed, and alternative modes of transport considered prior to new development being considered.*” (BAPDDP, p.59)

Section 9.3 of the BAPD Development Plan lists the following criteria for preparing a SDS Master Plan:

- a) *A community-wide public engagement process should be undertaken in the planning and development of SDSs.*
- b) *Shall be completed by a professional planner.*
- c) *The protection and restoration of buildings having historic significance should be encouraged.*
- d) *The protection of existing natural areas should be encouraged.*
- e) *Traffic access, site circulation, and traffic volumes should be considered and planned to ensure efficient movement of vehicles and compatibility with pedestrian routes.*
- f) *Parking levels, accessibility and location should be identified.*
- g) *Pedestrian and transit accessibility should be identified.*
- h) *Site landscaping will be required in accordance with the City of Brandon Landscape Design Standards.*
- i) *Compatibility of the proposed use, design, and scale of the buildings proposed with adjacent areas.*
- j) *Environmentally sensitive development including the buildings and landscaping, and the use of innovative technologies and materials that reduce energy consumption and waste will be encouraged.*
- k) *The various phases of development shall be identified and an estimated time for absorption of each phase determined. (BAPDDP, p.p. 59-60.)*

3.2 BRANDON GREENSPACE MASTER PLAN (2015): The 2015 Brandon Greenspace Master Plan (GMP) is a comprehensive plan to “*enhance the quantity, quality, function and amenities of the greenspace in Brandon now and in the future.*” (p.6) The plan recommended targeted improvements between 2015 and 2025 and guidelines and development concepts for the subsequent 10 year planning period to 2035. The GMP presented a new Greenspace Classification System along with a complete inventory and assessment of the full range of public active and passive recreational amenities and facilities, as well as the City’s parks and open spaces and ecologically significant and floodplain lands.

The GMP indicates that one golf course is adequate to service the current population. The Wheat City Golf Course is included as a component of the “Brandon Recreation Centre” (along with the rink, restaurant/lounge and tennis courts) in the Section 7.4 Greenspace Mapping Table and is identified as “Activity Greenspace” under the greenspace classification plan. The figures in the report code the golf course as “park” or “Existing Greenspace.” In the public online survey conducted for the GMP, 32% of respondents said they pleasure skated and 24% said they golfed and 3% identified the need for golf course improvements. Recommendation 3.4.6 *Maintain Recreation Centre Land* on page 17 of the Greenspace Master Plan recommends that the Wheat City Recreation Centre lands remain in public ownership and that the flood prone land be naturalized “if the use of the land changes.” The golf course also factors into the GMP’s plans for trial development alongside the adjacent CP tracks heading west and across the river to the north along 34th street, although the means for river crossing are not identified (Future Trail System, p. 55). Also, the golf course berm is identified as a segment of the trail system.

The GMP identifies the Brandon Recreation Centre for long term “naturalization” project under the Section 7.5 Projected Expenditures Mid/Long Term, which presumably refers to floodplain lands not used for golf course purposes. Finally, the GMP recommends that the 1995 River Corridor Master Plan be updated, which was subsequently accomplished with the completion of the 2016 Brandon Assiniboine River Corridor Master Plan referenced below.

3.3 BRANDON ASSINIBOINE RIVER CORRIDOR MASTER PLAN (2016): The severe Assiniboine River flooding experienced in 2011 and 2014 and the damage it caused to the river-bottom forest and active and passive recreational facilities located within the floodplain prompted the City to undertake the Brandon Assiniboine River Corridor Master Plan (BARCMP). The purpose of the plan was to re-envision the public lands within the river corridor as a major focus of urban recreation and to work with “nature” and the changing hydrology of the river in deriving plans for redeveloping designated activity zones located on public lands.

The study process was a community focussed, multi-stakeholder undertaking with an extensive public engagement program. While the BARCMP planning area encompassed all 700 acres of river corridor lands within the City, the major focus of the study was the middle portion of the corridor, mostly on the south side of the river with the exception of the lands containing the Riverbank Discovery Centre and Festival Park, running from Queen Elizabeth Park on the west at the foot of 26th Street to Optimist Park on the east bordering the east side of First Street. Detailed redevelopment plans were prepared and costed for five (5) activity zones within this core area while the river bottom lands beyond were accounted for in terms of: 1) hydrological impacts (water management and flood control), 2) riverbank access and transportation connectivity and, 3) river-sensitive re-vegetation efforts.

The Wheat City Golf Course and subject condominium site are located to the west of the core corridor zone and therefore peripheral to the major redevelopment plans. However, plans for redeveloping any portion of the golf course lands must take into account the BARCMP in terms of the three major planning and (re)development interest areas of hydrology, access/connectivity and (re)vegetation.

The BARCMP identifies the golf course and condo site as being part of the “Zone 2: River Use Zones and Access Points” area. By virtue of berm construction, the golf course is protected to the 2014 flood level with about 72 of the approximately 141 acres identified as having a “medium” flood risk, containing holes 1 through 13, and the remaining 69 acres having a “low” flood risk, containing holes 24 through 18 along with the club house, tennis courts, arena, cart storage building, and parking area. The subject condo site is likewise included within the low flood risk area as identified by the BARCMP.

3.4 CITY OF BRANDON ZONING BY-LAW 7124: The subject development site is currently zoned “PR” Public Reserve under the Brandon Zoning By-Law, while the golf course is zoned “A” Agriculture. To accommodate the proposed development and its principal and accessory uses, the site requires a rezoning from “PR” Public Reserve to “CG” Commercial General. It is also proposed to apply for a Conditional Use under the CG zoning district for a Planned Unit Development, which would exempt the project from the By-Law 7124 Table 12: Commercial Bulk and Siting Requirements as long as the Master Plan is able to demonstrate that the project will, in its entirety, exceed the minimum standards of the by-law and result in a higher standard of development in terms of site layout and landscaping, building design, and project amenities.

Several sections of the zoning by-law will apply to the subject development, as summarized below:

- Section 12: Conditional Use: A Planned Unit Development (PUD) is a Conditional Use under the CG Commercial General District. A Conditional Use application will therefore accompany the applications for rezoning and subdivision required by the development.
- Section 13: Public Outreach: Section 13 requires Public Outreach in connection with Conditional Use applications in addition to the public notice mandated under the Manitoba Planning Act. Essentially, residents potentially impacted by a proposed development must be notified, provided with detailed information on the development, and provided with an opportunity to comment on the development either verbally or in writing via a survey, door-to-door interviews, or at an advertised information meeting or public open house on the project.
- Section 44: Urban and Landscape Design Manual: Section 44 requires that developments comply with the City's Urban and Landscape Design Manual, Schedule C to Zoning By-Law 7124. The stated objectives of the design manual are:
 - *To create a strong community image by enhancing the character and quality of the built environment.*
 - *To encourage social, civic and physical activity.*
 - *To minimize the impact of development on the environment.*
 - *To provide efficient and convenient multi-modal transportation connections.*
 - *To facilitate safe and healthy living environments.*
 - *To integrate new developments which respect the scale, intensity, building and site design of the initial built form.*
 - *To alleviate adverse impacts on adjacent land uses generated by new developments through urban and landscape design.* (Schedule C, p. 1)

The standards and guidelines in the Urban and Landscape Design Manual deal with mobility and accessibility, amenity spaces, site access, circulation and parking, sustainable practices, railway proximity and project components related to the site, building and landscape design.

- Section 46: Municipal Services: Section 46 requires all development within the City to be provided with hydro, water, wastewater and land drainage. The water, wastewater and land drainage services can be private with the proviso that, at such time as City services are available, the development is connected within a specified length of time.
- Section 47: Streets and Lanes: Section 47 requires that all lots have frontage on a public street. However, Council can approve a building site on a private road connecting to a public street. Private roads servicing residential developments must be registered at Land Titles as a common element as defined in The Condominium Act. Private roads must also be paved.
- Section 48: Planned Unit Developments: As previously mentioned, Planned Unit Developments are a Conditional Use under the CG Commercial General zoning district. Conditional Use applications may be approved by Council if the applicant can demonstrate that the proposal:
 - *Will be compatible with the general nature of the surrounding area,*
 - *Will not be detrimental to the health or general welfare of people living or working in the surrounding area, or negatively affect other properties or potential development in the surrounding area, and*

- *Is generally consistent with the applicable provisions of the Development Plan, this (zoning) by-law and any applicable Secondary Plan.* (ZB p. 19)
- Section 53: Commercial Zones: The CG Commercial General zone provides for sites “*...at appropriate locations adjacent to residential zones, for a variety of commercial uses...*” (ZB p 53.) Residential apartments and licenced restaurants and lounges are permitted under the CG zoning. Outdoor recreation facilities like the Wheat City Tennis Club and Golf Course are a Conditional Use under the CG Commercial General zoning district.
- Section 69: Floodplain Overlay Zone: The floodplain zoning regulations govern the development of lands that fall wholly or partially within the 100/300 Year Flood Extents as identified on Reference Map 7 of the BAPD Development Plan. While the northerly portion of the site falls within the floodplain, the existing berm visible on Figure 2 will be heightened and extended to protect the proposed buildings. Moreover, the buildings will be constructed to comply with the flood-proofing measures stipulated in Section 69(d) of the zoning by-law. According to the City of Brandon Engineering Department, the grade around the building and the main floor elevation must be protected to a Flood Protection Level (FPL) of 361.7 metres ASL (1,186.6 ft. ASL) based on the flood-of-record (at the time of writing). The building grade at all four corners must therefore be at least 0.3 metres (1 foot) above the FPL, or 362 metres ASL (1,187.7 ft. ASL), while the Finished Floor Elevation (FFE) for the lowest residential floor or parking level in the apartment buildings must be at least 0.6 metres (2 feet) above the Flood Protection Level for the design flood, or 362.3 metres ASL (1,188.6 ft. ASL). The project engineer has indicated that the main floor elevation of the northerly building will be at least 364.5 metres ASL (1,195.9 ft. ASL) and possibly higher depending upon the final approach grade. In comparison, the elevation reached by the 2014 event was 361.2 metres ASL (1,185 ft. ASL).
- Section 71: Railway Protection Overlay Zone: The subject site falls within the Railway Protection Overlay Zone, the purpose of which *“is to ensure that development is compatible with railway operations as there is the possibility that the safety, health and welfare of residents could be adversely affected by railway activities.”* (ZB, p. 70) The residential component of the development complies with the zoning prohibition against residential development within 30 metres (100 feet) of a main line with a track speed of more than 65 KPH (40 MPH). The setback for the commercial development (arena extension) is 15 metres (50 feet). The arena extension will bring the corner of the building to about 3.0 metres (9.8 feet) from the rail ROW. The site plan was reviewed with the Real Estate division of CP Rail and, by email correspondence on file dated November 24, 2016, was approved on the basis that the setback would be sufficient for building construction and maintenance without encroaching upon rail property.

DEVELOPMENT COMPONENTS & LAND USE ANALYSIS

This section of the Master Plan provides additional detail on the major project components and how the project supports, implements or otherwise addresses applicable City of Brandon development policies and guidelines. Where relevant, development details, dimensions and quantitative data are presented for the major project components and principal uses in Figure 2: Site Plan. Figures 4, 5 and 6 show showing preliminary building elevations and floor plans, while Appendix “B” provides policy references and project compliance or proposed equivalencies for all the overall project and the individual components. It should be noted that the current site plan is subject to change depending upon the final site grading and building elevations, and the ultimate plan will require the approval of the Brandon engineering department prior to any development applications proceeding to public hearing.

4.1 RESIDENTIAL COMPONENT: The Riverside Estates residential component includes 68 units in two (2) condominium apartment buildings. The south building (Condo "B") is 4 storeys with heated main floor parking for 26 vehicles and 3 residential floors containing 4, 1-bedroom units and 20, 1-bedroom + flex units. The North Building is technically considered to be a 4 storey building but sits atop 2 levels of underground parking for 59 cars. The north building (Condo "A") contains 12, 2-bedroom units and 28, 1-bedroom + flex units and 4, 1 bedroom units. Surface, visitor parking near the two buildings is provided for approximately 19 vehicles on the east side and perpendicular to the drive aisle leading to the main door of the northerly condo building. The total residential parking supply is approximately 104 stalls.

The 1.047 ha. (2.59 ac.) parcel will be will be regraded and shaped and partially cleared to accommodate the condo buildings and paved areas. The north building is located about 30.5 meters (100 feet) back from the Ordinary High Water Mark (OHWM) of the Assiniboine River. The south building is located about 49 metres (160 feet) feet from the CP rail ROW and about 48 meters (157 feet) from the 4-plex at the SW corner of McDonald Avenue and 34th Street. Both buildings comply with "CG" Commercial General yard requirements and bulk regulations with the exception of the north building height. In the absence of the PUD Conditional Use application, a zoning variance would normally be required to permit the north building to have a height of 4 storeys instead of 3 storeys as permitted for multi-family buildings within the CG zone. Figures 4a to 4d show building elevations for the condominiums. Exterior cladding will consist of a combination of limestone and metal panels. The buildings will offer excellent river views from a majority of the units and the northerly building is provided with a roof-top terrace.

Figures 4a-4d: Residential Building Elevations

4.2 COMMERCIAL COMPONENT: The new commercial component consists of a 475.8 square metre (5,122 square foot) building on two (2) floors (2,561 sq. ft. per floor). It is proposed to place the building beside the arena immediately SW of the arena entrance. In addition to a new pro shop, the building will provide space for up to 7 commercial rental units. Figure 5 shows elevation drawings for the new commercial building.

Figure 5: New Commercial Building Plans

The second commercial component involves a 2-phase renovation of the existing arena consisting of a complete renewal of the entranceway, restaurant and lounge and the conversion of the curling rink to a regulation youth ice hockey rink. Restaurants and lounges are a permitted us in "CG" zones whereas "Sports Complexes" are a Conditional Use. In addition to better serving people using the full range of golf course and arena facilities, it is anticipated that the restaurant will be a popular feature for condominium residents as well as residents from the surrounding neighbourhoods. And the rink improvements will be beneficial to youth hockey in Brandon which must compete with juvenile, senior and recreation leagues for ice time at the City's full size arenas.

The arena building retrofit will involve a refurbished entry, restaurant, kitchen, lounge and washrooms, and support facilities on the second floor. The rink improvements will involve a new concession area, new viewing lounge and refurbished locker/change rooms on the lower level. The re-purposing of the curling rink into a hockey venue will include re-use of the existing ice plant, an approximate 15' easterly extension to obtain the required ice dimensions, a new Zamboni access, viewing stands and artificial and natural lighting improvements. There will also be upgrades to the building envelope, and mechanical and electrical systems to improve building energy performance and overall aesthetics and appearance.

EXTERIOR FINISHES
A - LIMESTONE
B - METAL PANELS



FOR ARTISTIC PURPOSES ONLY



Figure 4a
Residential Building Elevations

Elevations
Riverside Estates
3500 McDonald, Brandon, MB
HBL - NCA
A2.0

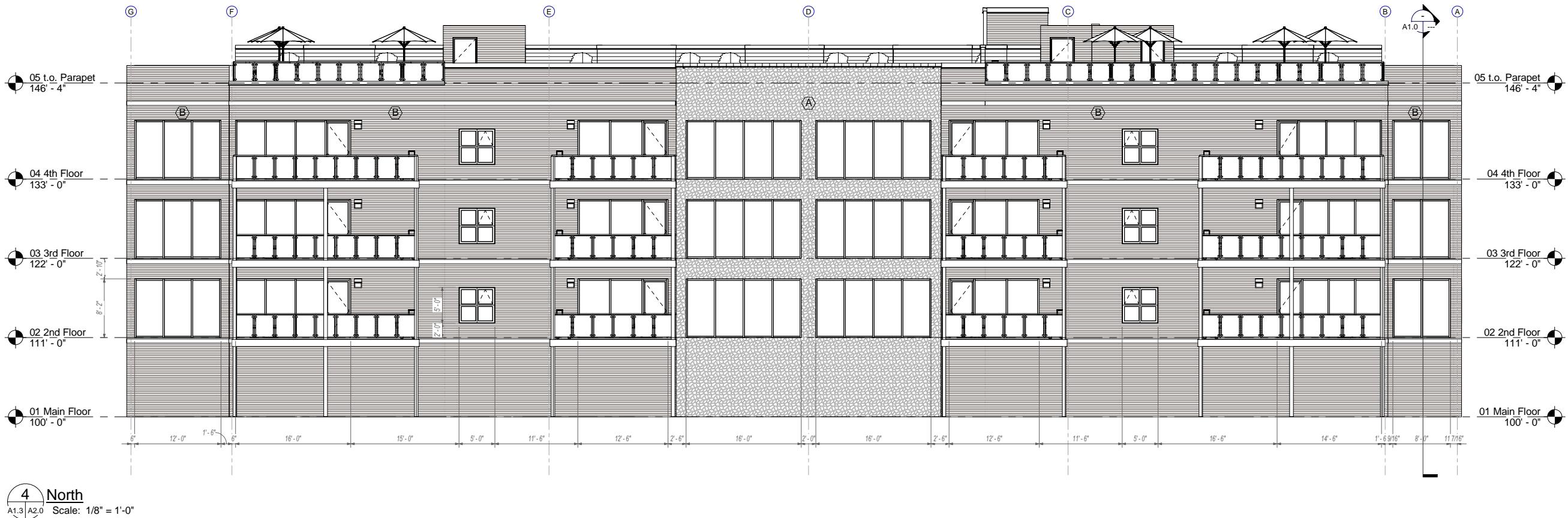


FOR ARTISTIC PURPOSES ONLY

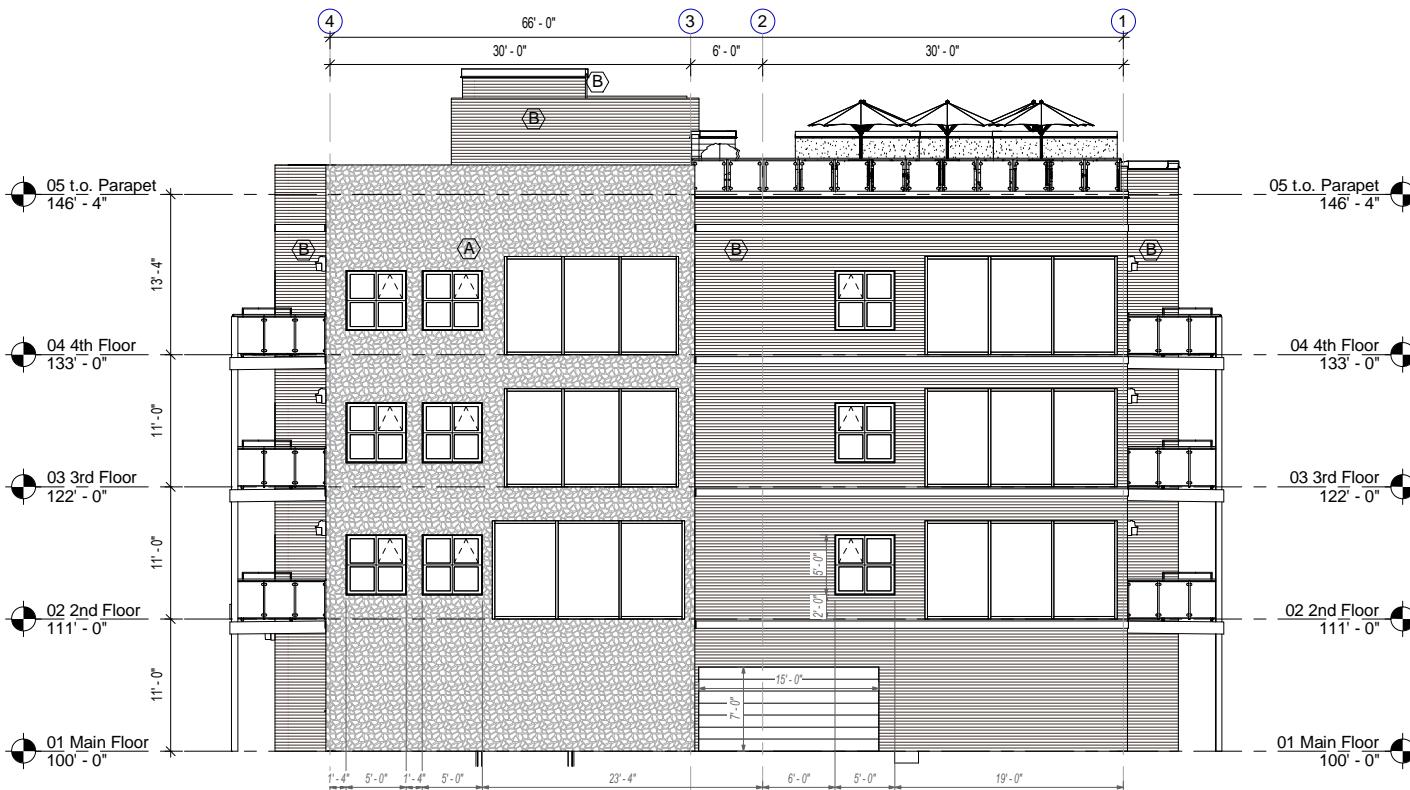


Figure 4b
Residential Building Elevations

Elevations
Riverside Estates
3500 McDonald Brandon, MB
HBL - NCA
A2.1



EXTERIOR FINISHES
A - TYNDALL STONE
B - CORRUGATED HORIZONTAL METAL



6 N.E. Perspective
A2.0 Scale:

Figure 4c
Residential
Bldg. Elevations

REV DESCRIPTION DATE / BY

SEAL: SEAL:

THIS DRAWING MUST NOT BE SCALED.
THE GENERAL CONTRACTOR SHALL VERIFY ALL DIMENSIONS, DATUMS AND
LEVELS PRIOR TO COMMENCEMENT OF WORK AND IS HELD RESPONSIBLE
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DRAWING:

Elevations

PROJECT:
Wheat City Golf Development
4 Story
3500 McDonald Brandon, MB

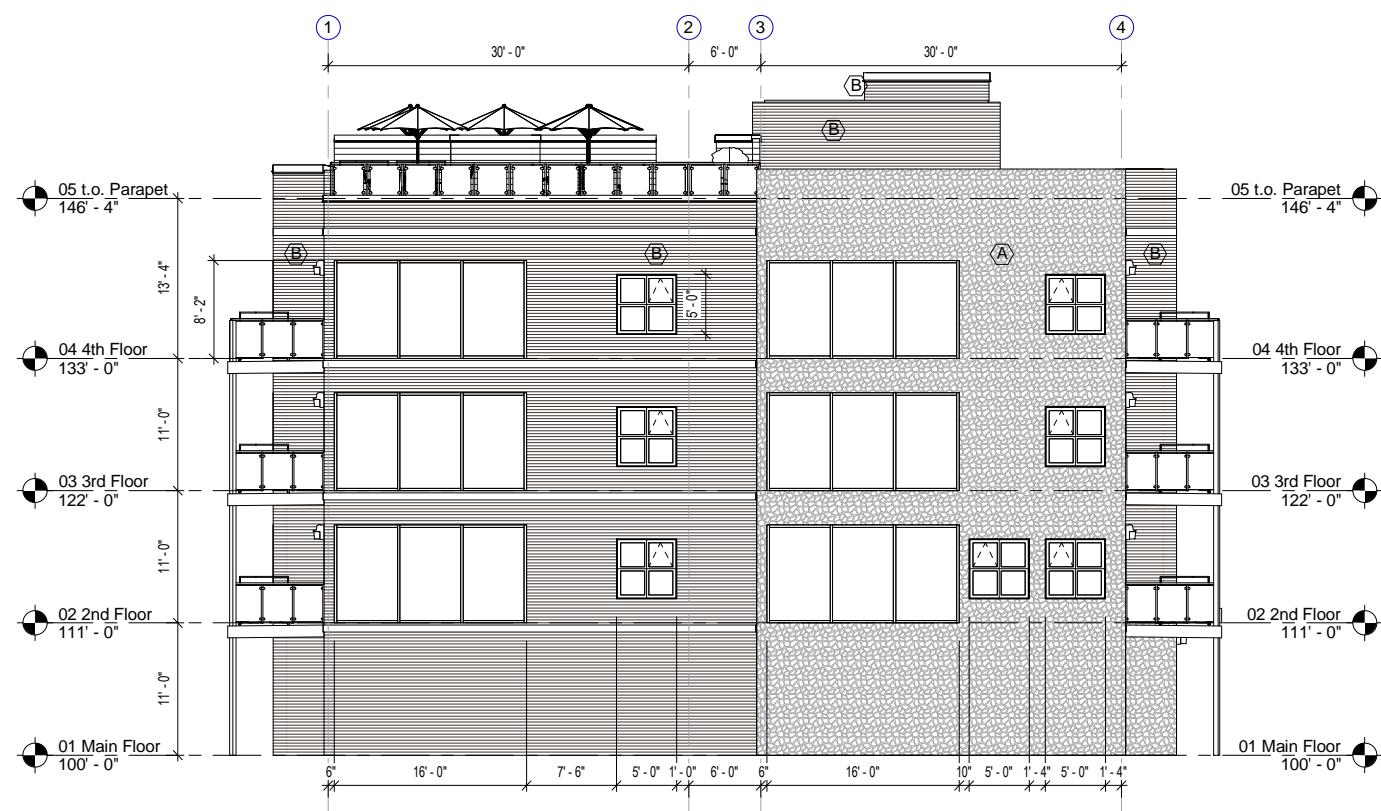
Neil Cooper Architect
374 River Ave #109, Winnipeg, MB R3L 0E4

Preliminary

DRAWN BY: LD	CHECKED BY: Checker	SHEET NO. A2.0	REV NO.
DATE: Mar2-17		SCALE: 1/8" = 1'-0"	
FILE: 17035			



1 South
S-100 A2.1 Scale: 1/8" = 1'-0"



2 West
S-100 A2.1 Scale: 1/8" = 1'-0"



3 S.W. Perspective
A2.1 Scale:

Figure 4d:
Residential
Building
Elevations

EXTERIOR FINISHES
A - TYNDALL STONE
B - CORRUGATED HORIZONTAL METAL

REV DESCRIPTION DATE / BY

SEAL: SEAL:

THIS DRAWING MUST NOT BE SCALED.
THE GENERAL CONTRACTOR SHALL VERIFY ALL DIMENSIONS, DATUMS AND
LEVELS PRIOR TO COMMENCEMENT OF WORK AND IS HELD RESPONSIBLE
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AS THE ARCHITECTURAL FIRM.

DRAWING:

Elevations

PROJECT:
Wheat City Golf Development
4 Story
3500 McDonald Brandon, MB

Neil Cooper Architect
374 River Ave #109, Winnipeg, MB R3L 0E4

Preliminary

DRAWN BY: LD DATE: Mar 2-17	CHECKED BY: Checker SCALE: 1/8" = 1'-0"	SHEET NO. A2.1 FILE: 17035
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The parking lot for the club house/arena facility will be expanded with the relocation of the cart shed to the area west of the arena. Access to the club house/arena remains unchanged however the expansion of the arena requires that a new road be constructed around the north and west perimeters of the facility to allow for public drop-off at the restaurant as well as access to the relocated cart storage shed. Figure 6 shows the floor plan for the lower level club house/arena improvements. The upper level improvements have not been finalized as they will require the input of the future restaurateur.

Figure 6: Arena Renovation Plan – Lower Level

4.3 CONNECTIVITY COMPONENT

4.3.1 Traffic Access & Parking: The redeveloped site will continue to be accessed from McDonald Avenue. However, the approach will have to be modified, in relation to grade if not location, at such time as 34th Street is extended northward across the tracks to connect with McDonald Avenue. The alterations will be triggered by having to increase the elevation of the 34th Street road bed as it descends to McDonald in order to minimize the street grade. Upon completion of the rail crossing, 34th Street will serve as primary access to the development. The access driveway will be shared by the Riverside Estates condominium project, the commercial components, and the golf course. A cross-access agreement between the corporate entities will be registered on the title of the respective properties to ensure unobstructed access and to govern matters such as maintenance.

At the time of writing, the site provides parking for approximately 269 vehicles for all residential and commercial components. The Parcel “A” residential component provides structure parking for 59 vehicles and surface parking for 19 vehicles, while the Parcel “B” commercial components provide surface parking for approximately 165 vehicles. Under the City of Brandon zoning by-law, the development would require a total of 305 parking stalls based on the land use, the number of residential units, and the commercial square footage. However, the City has required the provision of 70 parking stalls dedicated to the golf course, which boosts the development parking requirement to 375 stalls, which amounts to a theoretical parking shortfall of 106 stalls. It is also noted that, to avoid impacting the parking on Parcel “B” upon the construction of the 34th Street rail crossing, a retaining wall will be required along the west limit of the elevated road bed, parallel to the Parcel “B” property line between the railway tracks and the site approach.

Despite the by-law shortfall, in situations such as this where different uses share parking, the City administration may accept the findings of a Parking Management Plan if it is able to demonstrate that the amount of parking the site is able to provide is adequate to support the overall development based on the shared-use of available parking between the different uses. This is the case, for instance, where the peak demand for parking for one use coincides with a reduced demand for parking for another use. Thus, as regards the subject site, the golf course parking demand off-sets the arena parking demand based on the seasonal nature of these uses. A Parking Management Plan was prepared for the Wheat City golf course redevelopment project and is included as Appendix I to this master plan, which demonstrates that the available parking is adequate to meet the needs of the overall development based on shared use with the future development of approximately 50 stalls on City-owned land south of the tracks.

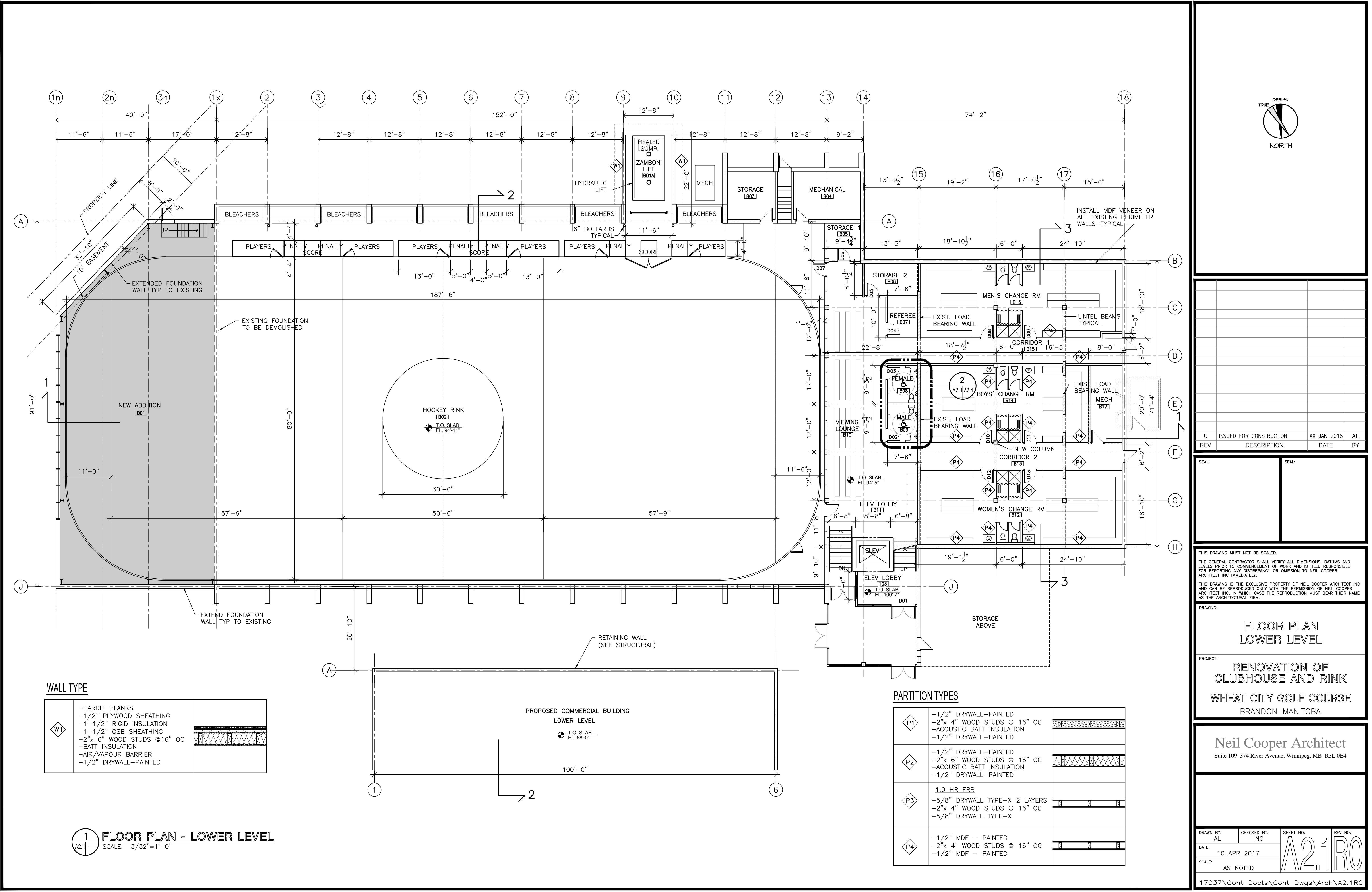




Photo: View looking west down First Fairway with new berm.

4.3.2 Transit: Currently, there is now bus service to the Brandon Recreation Centre and Wheat City Golf Course. However, with the proposed mixed-use development, it is anticipated that transit service will be required to enable customers and residents to access services and amenities in the downtown and across the City.

4.3.3 Active Transportation: The Wheat City Golf Course lands contribute to different modes of Active Transportation and factor into recommendations for recreation improvements in both the 2015 Greenspace Master Plan and the 2016 Brandon Assiniboine River Corridor Master Plan. Figure 7a is an excerpt from the Greenspace Master Plan (GMP, p.55) showing the proposed Future Trail System.

The solid green lines in Figure 7a are existing trails which, in the vicinity of the subject site, include a primary trail running south on 34th Street and east on Pacific Avenue and a secondary trail running west from Pacific Avenue along the north side of the CPR mainline. The dashed lines indicate future trails including a northerly extension of the 34th Street primary trail across the Assiniboine River to eventually connect with proposed and existing east-west trails through the floodplain and along Grand Valley Road, as a way of interconnecting the Inner Loop and Outer Loop trails.



Figure 7a: GMP Future Trail System

Figure 7b is an excerpt from the 2016 Brandon Assiniboine River Corridor Master Plan (CMP p. 57) showing what is envisioned for the golf course lands in terms of Active Transportation. The solid green line shows an “Informal Trail” along the berm constructed to protect the golf course from extreme flooding. The dashed green line shows cross country ski trails that are groomed on the golf course. The yellow links are identified in the report graphic as “previous/potential pedestrian and ferry crossings.” The solid red line down 34th Street and across Pacific Avenue is an “active fully developed A.T. route”

while the dashed red line along McDonald Avenue is identified as a “missing link to close routes.” The letters refer to a Concession Stand and Washrooms.

The on-site pedestrian walkway along the main access road will interconnect with the existing sidewalk on the north side of Macdonald and the sidewalk on the west side of the 34th Street extension. These walkways ultimately connect with the Assiniboine trail on the golf course berm and will pass by the Assiniboine River Natural River Bank Heritage area identified in the City of Brandon master Plan. Internal walkways link all areas of the site with those adjacent to parking/roadways will be hard-surfaced and raised on curbed boulevards. Pedestrian crossings of vehicular ways and parking areas will be marked with textured hatching or raised with roll curbing if required.

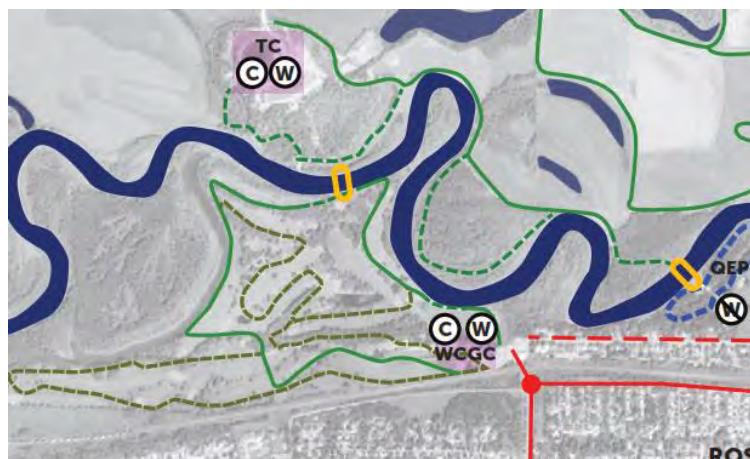


Figure 7b: CMP Active Transportation Map

Bicycle use will be encouraged and the site will be speed-restricted to minimize danger to cyclists. Bicycle parking will be provided for the condominiums utilizing recognized standards of a minimum of one (1) bicycle stall for every ten (10) parking spaces. Bicycle parking will also be provided at the clubhouse and commercial building. The 2015 Greenspace Master Plan states that a dedicated bike lane on McDonald would improve access to the BRC.

4.3.4 Universal Accessibility: The site and building will comply with zoning regulations concerning the provision of accessible parking stalls (1 accessible stall per 25 stalls) and building code regulations concerning barrier-free access to all venues. In particular the residential buildings and suites will be designed in accordance with City and Provincial visitable and accessible standards as confirmed during the City’s site plan review and architectural plan examination process leading to the release of a development permit and occupancy permit. The site will be provided with sidewalks with mountable curbs along access roads and parking lot walkways or equivalent pavement markings indicating pedestrian access to building access points. New and renovated commercial spaces will abide by building code regulations concerning accessible washrooms and public areas. The club building will have direct grade access to a vestibule after which stairs split half-flight up to restaurant/lounge and half-flight down to rink viewing/locker areas. A lift will be provided for barrier-free access to the levels.

4.3.5 Traffic Impact Study: A Traffic Impact Study (TIS) was prepared for the project and sealed by a qualified traffic engineer (see Appendix C). At the time, the development was planned for 72 residential units, as opposed to the current 68. The TIS examined the impact of increased site traffic on the public street system and the intersection of 34th and McDonald and downstream intersections.

Understandably, there are low traffic volumes moving through the 34th and McDonald Avenue intersection currently, amounting to 49 trips during the PM peak hour. Under full development, the current project was, based on the initial proposal, expected to generate 617 AM weekday peak hour trips (and 173 entering and 444 exiting) and 635 PM weekday peak hour trips (388 entering and 247 leaving). Total all-day weekday trips entering and exiting was estimated at about 3,000 rounded. While this represents a substantial increase over current volumes, when prorated over a 12 hour day, it amounts to about 4 vehicles passing through the intersection per minute.

The study estimated that the extension of 34th will essentially split the traffic with 50% entering and exiting to and from the north and 50% entering and exiting to and from the east. The study modelled the impact of the traffic on the 34th and McDonald intersection as well as downstream intersections and concluded that there is sufficient capacity at all intersections in the study area to accommodate existing and development traffic at full build-out in 2020. With the additional traffic, the intersection level-of-service (LOS) for the modelled intersections did, however, drop from LOS A, the best rating (very low delay) to LOS B (low delay). To put this in perspective, the intersection ratings run from LOS A to LOS F (fail).

The traffic will likely be spread out over the day due to the type of recreational facilities at the BRC and the assumption that most of the condominium residents will be retirees. On the basis of golf course memberships and walk-ons, the existing golf course traffic is expected to remain at current levels. The additional traffic generated by the new hockey venue is not cumulative as it will largely occur outside of golf season as there are no plans for a summer program. The restaurant traffic will largely be off-peak traffic and, therefore, less noticeable. As the commercial units will likely be occupied by retail and personal services in a symbiotic relationship with the recreation facilities, an estimated 30% the traffic to the units will already be on site. Traffic generated by the condominium units will account for the largest portion of new traffic.

4.4 PARKS & RECREATION COMPONENT: Under the 2016 Assiniboine River Corridor Master Plan (CMP), the golf course is identified as part of the “River Use Zone and Access Points” on the Figure 20 Site Assessment Location (p.71). The main priorities for this zone are 1) hydrology, 2) transportation and river access and, 3) river-first vegetation zone. Elsewhere in the CMP, the BRC is identified as a “pay-for-use area.” That is, no additional, major public recreational facilities are identified for the zone which spans the western reaches of the river corridor west of the major corridor redevelopment zones north and east of the downtown and “The Flats” transitional zone. That said, the golf course is a City-owned public venue for golf, tennis and cross-country skiing and public access to the river trail will be facilitated through the subject development.

The public workshops conducted for the 2015 Greenspace Master Plan queried participants on necessary recreational and related improvements. Comments related to recommended Brandon Recreation Centre improvements included:

- Area should be naturalized
- Additional entertainment facilities should be considered
- Location for additional sports facilities
- Updated park facilities
- Maintenance needs
- Additional park facilities
- Parking lots needed
- Enhanced (AT) connections needed

The Wheat City Golf Course is identified as a Plan Driver in Chapter 5 Plan Drivers of the 2015 GMP. Major stakeholders included the City, golf leagues, tennis clubs, the Turtle Crossing Campground, private developers, restauranteurs and the general public. Future “possibilities/desires” included the following reference: *“Redevelop clubhouse and find a high-end operator for the restaurant partner with a private developer to add high density 55+ housing adjacent to or on the course.”* (CMP, p. 92)

4.5 SITE DEVELOPMENT AND LANDSCAPING

4.5.1 Site Landscaping: The major site design and landscaping features are illustrated in the Figure 8 Conceptual Land Use Plan. Given the size and configuration of the condominium parcels site as well as the slope constraints, disturbance of the existing site and vegetation cover by the proposed development is unavoidable. In particular the site will be regraded and shaped so as to stabilize the slope while raising the northeast portion above the level of the berm. This will involve removing a portion of the riverbottom forest although previous flooding has damaged the forest in the ravine (34th ROW primarily) to the point where many trees require removal irrespective of the development. The condominiums and commercial building have been sited to take advantage of the views offered from the site to the golf course and the river.

The site will be planted with trees and shrub and flower beds. Amenity spaces will be provided throughout the development interconnected with hard-surfaced walkways in a landscaped setting including grassed areas, shade and decorative plantings, and common terrace areas at river and valley viewpoints. Outdoor amenity space for the condominium buildings is shown on the Figure 2 Site Plan to the east of the 1st T, across the cart path in the vicinity of the existing putting green. The Brandon Recreation Centre will also be provided with additional amenity space to the west where a pergola covered seating area is to be provided between the outdoor restaurant deck and the tennis courts.

Signage will be limited to low profile monument signs for the condominiums and the club building integrated with the site landscaping. The commercial building will have discreet facia signage. Refuse bins will be concealed with fencing or vegetation screens and designed to integrate with landscape elements. The parking areas will also be provided with landscaped islands serving as bioswales for filtering stormwater runoff. No new wooden or wire fencing is contemplated for the site. Instead, hedging and plantings will demarcate the condominium parcel from the balance of the redevelopment site and public use lands.

In terms of safety and security, the site elements will be disposed so as to avoid tight, unobservable areas. Lighting will be provided throughout and surveillance cameras will be deployed. Site lighting will be down-cast side-shrouded to obviate light pollution and pedestrian paths will be lit with bollard or short, shrouded fixtures.

Figure 8: Conceptual Landscape Plan

4.5.2 Environmental Design Considerations: Low Impact Design (LID) elements and considerations were taken into account, where relevant, in the site and building design. It is the intent of the designers to maintain the existing landforms as a historical reference and to work with the existing topography where possible in order to maintain natural drainage patterns. The City of Brandon's LID project checklist is attached as Appendix D. Key LID elements are discussed below:

- Flood Mitigation: The development has to be protected to at least a 1:100 year design-flood protection level using berm protection and/or site filling. The existing clubhouse, new commercial and south condominium building are protected due to their elevation position. The



north condo building will be set on an elevated site that will tie into the berm extension. The site build-up will be accomplished with engineered deposition of fill under the buildings and hard surface areas and where repose slopes are required to support the build-up. This area will, by necessity, encroach into the existing ravine primarily located within the 34th Street North ROW.

- Water Management: Site drainage from hard surfaces will be channeled via curb and gutter construction into bioswales on site or on the golf course prior to entering the Assiniboine River. The main parking areas will have landscaped islands using French Drain construction techniques designed to collect water for filtration prior to seepage into the underlying soil. Elsewhere, xeriscaping concepts will be employed for “soft” landscaping design.
- Erosion Control: Erosion control measures will be implemented and existing sloped areas within the site, which are currently landscaped and stable, will be preserved. New slopes, roadway and condominium site build-up and associated repose slopes will be constructed in accordance with the geotechnical and slope stability analysis and related engineering design recommendations. Erosion control will be ensured through the installation of riprap protection atop nonwoven geotextile fabric along the river embankment of the site as per the slope stability report or an alternative accepted by the City. All exposed slopes will be seeded and the riverbank will remain undisturbed to as great an extent as possible. The site geotechnical and slope stability analysis is attached as Appendix F.
- High Infiltration and Hydric Soils: Construction on seasonally saturated, anaerobic soils will be avoided. Existing drainage systems, which are effective and benign with respect to soil saturation, will be preserved. This includes the ravine drainage channel, the natural drainage course from the southern portion of the site that outfalls onto the floodplain near the river, and the existing golf-course drainage and retention pond. Additional information on the site drainage is provided in the civil engineering report attached as Appendix E.
- Sustainable Building: New buildings will be energy-efficient and designed to exceed the National Energy Code. The existing club building will be retrofitted with new roofing, insulation, energy-efficient glazing and energy-conserving mechanical and electrical systems.
- Site Disturbance: The Developer intents to minimize disturbance and maintain the existing character of the site and adjacent golf course as much as possible, as it is in the interests of the development to maximize the natural elements and intrinsic qualities of the site. The golf course will be altered slightly in the first tee area. The “hill” separating the main parking lot and club building from the golf course proper will be left as is with the exception of a small ramp installed to convey golf carts safely down from the upper level. This landscape cut will be stabilized with retaining walls which will not intrude on the upper level views. The areas where the topography will be altered are the road extension between the club building and the existing tennis courts, and the north condominium site. The road extension is necessary to enable maintenance vehicles and fire trucks to access the golf course maintenance facility while the condominium site must be built up for purposes of flood protection. However, this build-up is topologically an extension of the diking system just recently installed by the City.
- Site Re-vegetation: Many of the existing trees within the development area will be preserved with the exception of trees in the east ravine area where slope regrading, foundation excavation and fill deposition will occur. As previously noted, a majority of the trees within the floodplain are dead or their health compromised due to the reoccurring flooding in recent years. Removed

vegetation will be replaced with a mix of native deciduous and coniferous trees, shrubs and ground cover to enhance the site's natural appearance. The objective is to maintain the natural character of the river bank adjacent to the development through the propagation of native vegetation.

4.6 SITE SERVICING COMPONENT

4.6.1 Servicing Study: A municipal servicing study was completed for the development by a qualified engineer and is attached as Appendix E. The study has been updated to reflect the most recent development proposals. Figures 9a and 9b illustrate the site sewer, water and land drainage servicing based on the updated site plan.

Figure 9a Water and Sewer

Figure 9b Land Drainage

4.6.2 Wastewater Collection: The municipal servicing study errors on the side of caution in terms of estimating project occupancy (2.5 persons per unit) and water consumption from residential, commercial and recreational use. The rink building and new commercial building will be serviced with a 200mm gravity sewer. The residential buildings will be serviced with pumps and 100mm pressurized mains connecting to a manhole on the 200mm line connecting to the existing manhole at the west end of McDonald Avenue. The McDonald sewer is also a 200mm line and has sufficient capacity to readily accommodate sewage flows from the development.

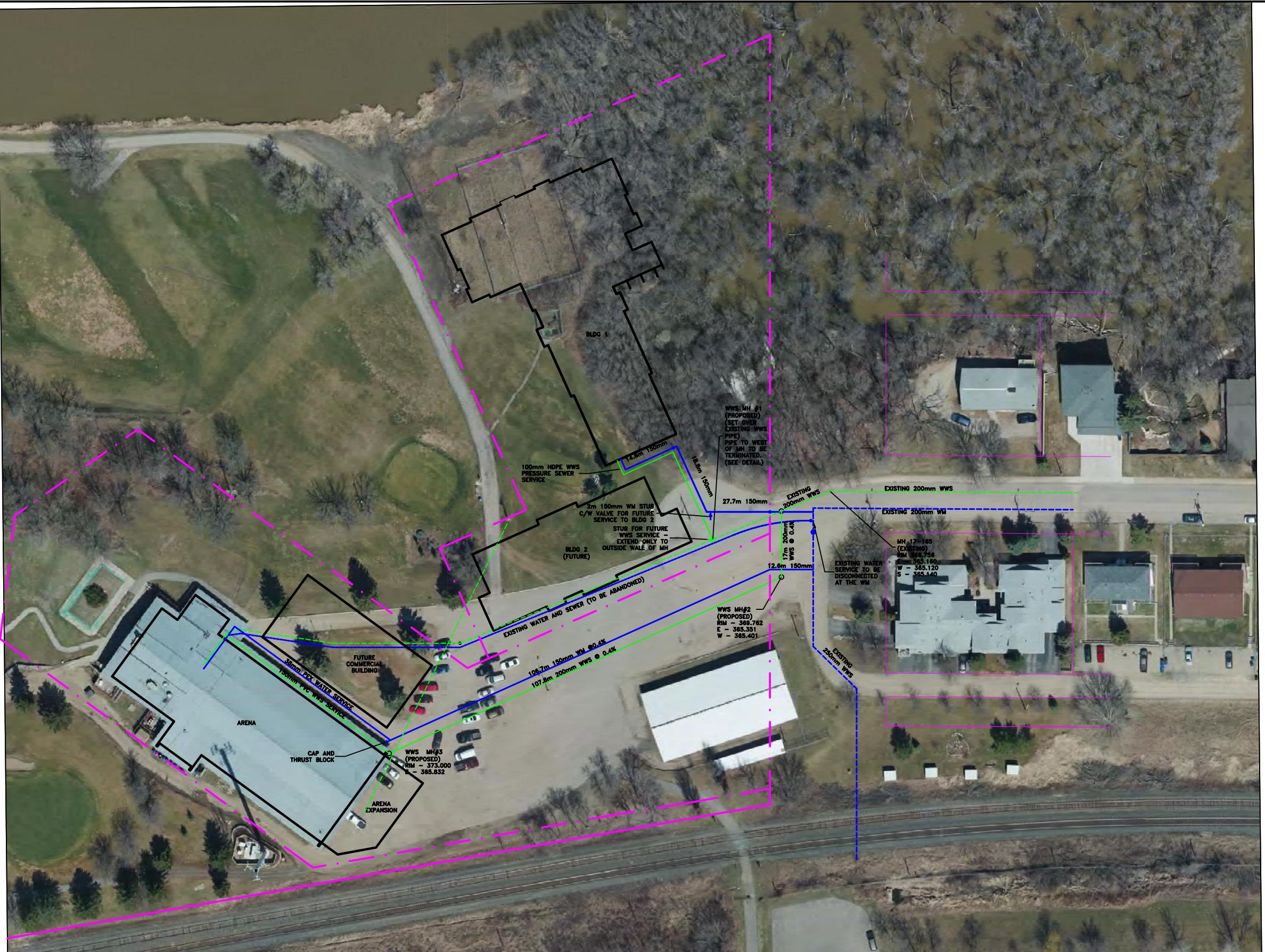
4.6.3 Water Supply: The development will be serviced with a 150mm watermain connected to the City's 250mm main in McDonald. Two hydrants will provide fire protection to the development. Water pressure and line capacity will be sufficient to service the development and provide fire flows.

4.6.4 Stormwater Management: In order to calculate the on-site storage and discharge requirements, the modelling used both City of Brandon and Provincial design criteria. For runoff discharging to its land drainage sewers, the City limits post-development discharge from a 100 year storm event to a pre-development discharge from a 100 year event. Manitoba Sustainable Development and Manitoba Infrastructure design criteria limits the post-development discharge rate for a 1:25 year event to the pre-development discharge rate for a 1:5 year event.

For purposes of land drainage modelling, the development site was divided into 3 drainage zones based on the current drainage characteristics (Appendix E, Drawing 6). Essentially, Area 1 includes the parking lot which currently drains to an on-site land drainage sewer (LDS) that connects to the McDonald land drainage sewer. Area 2 drains to the NE down the escarpment to the river while Area 3 drains to the golf course. The development's land drainage system will be designed to maintain this drainage pattern but with greater control in order to meet the City and Provincial stormwater runoff design criteria.

Necessary on-site storage and runoff retention will occur using paved surfaces, curb and gutter channeling, rooftop leads and catch basins and laterals while runoff filtration and infiltration will be achieved using bio-swales, soft landscaping, permeable surfaces, undisturbed natural drainage features and the golf-course drainage swales and first fairway storage pond which pumps water into the river when full. Site drainage methods will be designed to meet both City and Provincial design criteria.

4.6.5 Solid Waste Management & Recycling: The City will collect solid waste and recycled material from the existing BRC club house and new commercial building while the condominium project will contract with a private collection firm.



BENCHMARK

ALL DIMENSIONS ARE IN METRES
ALL ELEVATIONS ARE IN METRES ABOVE SEA LEVEL

LOCATION OF UNDERGROUND STRUCTURES
APPROXIMATE ONLY. EXACT LOCATION MUST BE
FROM THE INDIVIDUAL UTILITIES PRIOR TO CONSTRUCTION

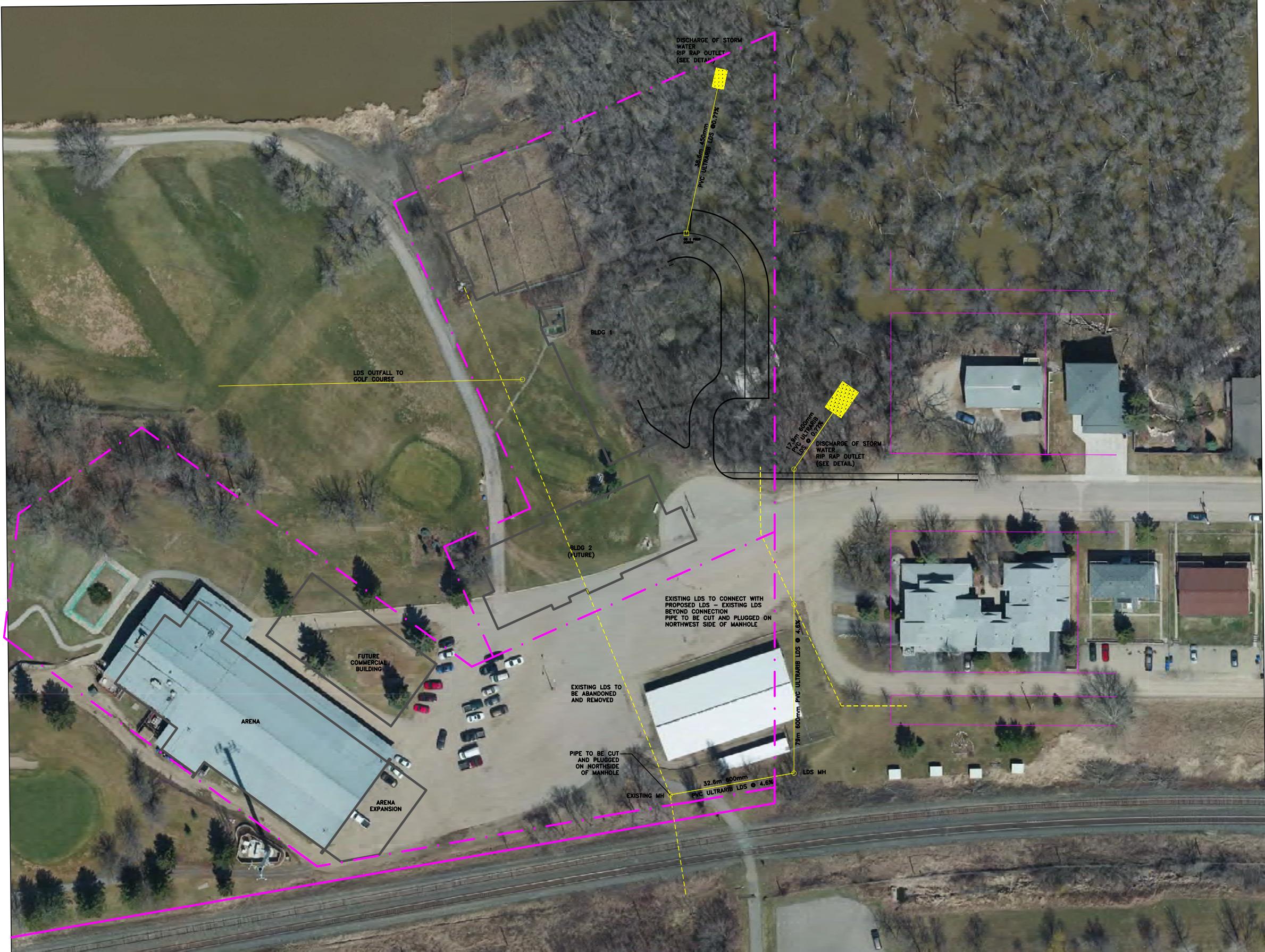
REVISION

G.D. NEWTON AND ASSOCIATES INC.
727A 10TH STREET
BRANDON, MANITOBA
R7A 4G7

RIVERSIDE ESTATES
500 McDonald Brandon, MB

Figure 9a
Water and Sewer

DATE 2018/03/19



BENCHMARK

ALL DIMENSIONS ARE IN METRES
ALL ELEVATIONS ARE IN METRES ABOVE SEA LEVEL

LOCATION OF UNDERGROUND STRUCTURES ARE APPROXIMATE ONLY. EXACT LOCATION MUST BE OBTAINED FROM THE INDIVIDUAL UTILITIES PRIOR TO CONSTRUCTION.

REVISIONS

G.D. NEWTON AND ASSOCIATES INC.
727A 10TH STREET
BRANDON, MANITOBA
R7A 4G7

DATE 2018/03/19

RIVERSIDE ESTATES
3500 McDonald Brandon, MB

Figure 9b
LAND DRAINAGE

4.7 GEOTECHNICAL ANALYSIS: The findings of the geotechnical analysis indicated that the site is buildable if the recommendations concerning foundation design and site regrading and slope stabilization are adhered to and the work properly monitored.

4.7.1 Geotechnical Analysis: The site Geotechnical report was prepared by a qualified engineer and is attached as Appendix F. A total of 16 test holes were augured at locations corresponding to the siting of the proposed commercial and residential buildings and the material was removed and sent for testing to determine the soil profile. Refusal was encountered at less than 50 feet in 8 of the 16 holes, with the shallowest hole at 20 feet. Groundwater was encountered in 9 of the 16 holes with severe sloughing and groundwater infiltration. Except where fill had been deposited, the soil profile was fairly uniform across the area consisting of “grey, very stiff to hard, damp, dense to very dense, clayey sandy SILT with cobbles and boulders,” commonly known as Glacial Till. Foundation recommendations ranged from concrete footings and bored or cast-in-place concrete piles for the commercial building to driven, pre-cast piles or treated Douglas Fir timber piles for the residential buildings. Recommendations for sidewalk and asphalt paved areas were also recommended and emphasized the need to excavate previous fill or silty soils where encountered.

4.7.2 Slope Stability Analysis: The Slope Stability Report is attached as Appendix G. Based on the topographic survey, there is a drop of 12 metres (40 feet) from the club house down to the floodplain. Slopes range from a gradual 6:1 to a steep 1.5:1. Slope stability expressed in angle of friction ranges from a low of 15-25 degrees where sandy-silt fill has been deposited to 20-25 degrees atop river-deposited clayey soil, to 40-45 degrees atop the undisturbed, highly cohesive glacial till. Based on the analysis, it was recommended that slopes in excess of 5:1 be stripped of top soil and flattened and reformed at 5:1 using crushed rock and granular base material deposited and compacted in lifts. Slopes susceptible to river erosion are to be flattened and rip rap placed atop nonwoven geotextile fabric from the Flood Protection Level down to the winter water level.

PUBLIC OUTREACH

5.1 METHODOLOGY: In March of 2017, the project proponent, Horizon Builders, delivered letter to residences within 100 metres of the development. The letter introduced the project, highlighted its major components using an attached site plan, and invited recipients to attend a community meeting at the BRC clubhouse on April 10th, 2017. Three (3) residents from 2 households attended the meeting along with the Ward Councillor and representatives from Horizon Builders. The letter and meeting summary are attached as Appendix H while the Figure 10 table summarizes the points raised in the meeting and the proponent's responses.

Figure 10: Resident Feedback Summary

Item	Feedback	Proponent Response
1	More people should have been invited.	Followed by-law requiring notification radius of 100 metres.
2	Walking paths should be enhanced and their use encouraged for walking, biking, snow shoeing and cross country skiing.	Agreed. Will work with City to improve and promote pathways and erect signage warning of errant golf balls.
3	Fate of golf cart storage shed.	Relocated west of club house.
4	Status of 34 th street crossing raised by condo residents at 3350 McDonald. Concern about driveways built atop the ROW. Has CPR agreed?	Timing of 34 th extension up to City. Design will have to accommodate continued access to the condo garages. CPR has given approval for level crossing.
5	When will construction begin?	Depends upon pre-sales and timing of approvals, but possibly Fall of 2017 or Spring of 2018.
6	Will there be a pro shop in the new commercial building?	Possibly a pro shop but discussions are underway with different potential tenants.
7	Will golfers hauling carts on trailers be provided with parking?	Will consider car-trailer pull-through stalls if space allows.
8	What about the restaurant?	Will be renovated and re-opened.
9	Questions posed concerning the condo buildings and units and opportunity for further input. Pleased the course will be repaired and flood protected.	People can address Planning Commission at the Public Hearing for the PUF conditional use and the rezoning and subdivision, but willing to hold another meeting if requested.

5.2 FEEDBACK & RESPONSE: Overall, the feedback was positive. The major concern was the impact of the extension of 34th on the driveways leading to the 4-unit condo at the SE corner of McDonald and 34th Street North. The design of the 34th Street rail crossing will have to accommodate continued access to the condo parking garages, possibly via a new lane running alongside the 34th Street extension connecting McDonald Avenue to the existing lane to the south and rear of the condo property.

DEVELOPMENT STAGING

The following project schedule is tentative, based on the development approvals timeframe and condominium pre-sales.

Phase 1: July 2018 – May 2019

- 1-A: Spring 2018: Upon project approvals, general site development including sewer and water mains, access roads and site preparation for Condo Building “B” (south condo building).
- 1-B: Renovation to existing club house building including the conversion of the curling rink to a hockey rink, renovations to lower level lockers, viewing area and washrooms and demolition of main level kitchen and restaurant/lounge.
- 1-C: Summer 2018: Commencement of construction of Condo Building “B” and continuation of general site development.
- 1-D: Commencement of renovation to the existing club building including building envelope, restaurant/bar/washrooms, lockers and viewing on the main level.
- 1-E: Fall 2018, construction of commercial building to be started with completion slated for early summer 2019.

Phase 2: May 2019 - May 2020

- 2-A: Site development including emergency access and maintenance access road on site. Construction continuing on south condo building and commencing site improvements in preparation for Condo Building “A” (north condo building).
- 2-B: Exterior upgrades to existing club house building completed.
- 2-C: Completion of south condo building and commercial building & related parking and amenities.

Phase 3: May 2020 – September 2021

- 3-A: Completion of site preparation improvements for Condo Building “A” (north condo building).
- 3-B: Commence construction of north condo building and related parking and amenities.
- 3-C: Completion of all construction and site landscaping by September of 2021.